

CALIFORNIA MOD
LITE
ASSOCIATION
(CMLA)

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The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules. No expressed or implied warranty of safety shall result from publications of or compliance with these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator or official. The race director shall be empowered to permit reasonable and appropriate deviation from any of the specifications herein or impose any further restrictions that in his opinion do not alter the minimum acceptable requirements. No expressed or implied warranty of safety shall result from such alteration of specifications. Any interpretation or deviation of these rules is left to the discretion of the officials and their decisions are final.

1 PREFACE

The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules. No expressed or implied warranty of safety shall result from publications of or compliance with these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator or official. The race director shall be empowered to permit reasonable and appropriate deviation from any of the specifications herein or impose any further restrictions that in his opinion do not alter the minimum acceptable requirements. No expressed or implied warranty of safety shall result from such alteration of specifications. Any interpretation or deviation of these rules is left to the discretion of the officials and their decisions are final.

2 GENERAL RULES

2.1 All vehicles are subject to inspection at any time.

2.2 Approval of a vehicle by the inspector shall mean only that the vehicle is approved for participation in a competitive event and shall not be construed in any way to mean that the inspected vehicle is guaranteed mechanically sound. Be it further declared that the inspector should not be liable, nor shall the sanctioning body, for any mechanical failure not for any losses, injuries or death resulting from same.

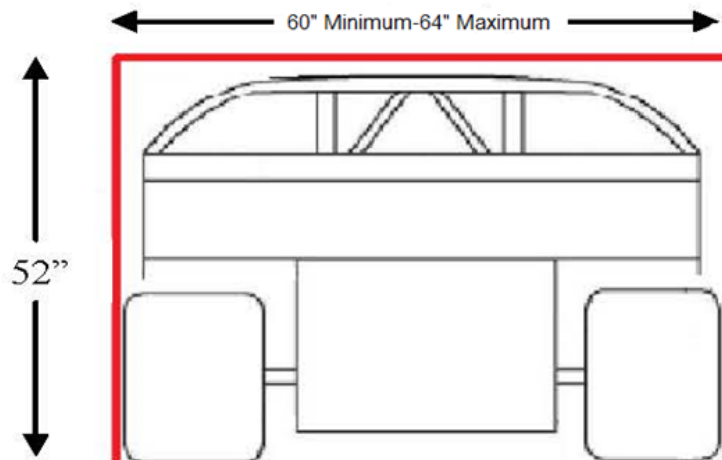
3 CLASSIC MODIFIEDS & MODIFIED LITE (IMCA/UMP/DIRT) SPECIFICATIONS

3.1 Any car specifications not covered in these body specs are subject to CMLA officials interpretation. Any significant performance advantage will be addressed on an individual basis. CMLA official's reserves the right to determine what constitutes an unfair advantage.

3.2 All chassis specifications are the same for all body styles.

3.3 All measurements may be taken with or without the driver and/or with/without fuel tolerance permitted on all body dimensions is maximum of +/- (plus or minus) 1/2" -inch (One half inch). This is a tolerance, not a dimension that is intended to be added to the body dimensions.

3.4 The intent of these specifications is to follow the bodyline and design of an exciting IMCA/UMP/DIRT modified racecar. Body must be 5/8 scale, stock appearing. Maximum outside tire width 64", Cars must pass freely through a hoop 60" wide-64" maximum by 52" high, 73" wheelbase plus or minus 1".



3.5 NO car over 52" tall, top to ground. Body height maximum 44", bottom edge to top (including frame rail).

3.6 Maximum body width: 64" Maximum frame width 34" min 30."

3.7 Body contour must follow frame and roll cage. Roof hatch entrance allowed.

3.8 Driver's door must be operational and equipped with secure latch.

3.9 Fasteners on hood, trunk, or panels must be positive, locking type.

3.10 128" maximum total length, bumper to bumper.

3.11 Kick outs or side nerf bars must be mounted two or three points and run full length between tires. 1 1/4" max .125 wall thickness. Cannot stick out beyond 1" from the side of the tire.

4 MODIFIED LITE (IMCA/UMP/DIRT) BODY SPECIFICATIONS

4.1 84" max - Door and rear quarter length.

4.2 30" min 38" max - side height of rear quarter panel from the ground. 30" max height of quarter panel.

5 MODIFIED LITE (IMCA/UMP/DIRT) HOOD AND NOSE SECTION

5.1 20" min 27" max - hood height excluding breather protectors (bottom of frame rail to top of hood).

5.2 Rear of hood becomes part of windshield area rules.

5.3 No part of nose section (in front of spindle centerline may be wider than a maximum of 24").

5.4 Hood may taper out to door panels at the rear.

5.5 Maximum 1.5 inch side fins allowed on aluminum nose.

5.6 Maximum 1.5 inch side fins allowed on aluminum nose

5.7 IMCA/UMP Body only - approved plastic nosepieces allowed. Plastic nose must be mounted in an approved manner and can extend no further back than front of hood.

5.8 Cooling holes allowed

6 MODIFIED LITE (IMCA/UMP/DIRT) ROOF PANEL (Figure #5)

6.1 40" min 48" max - length of roof panel. 36" min 42" max - width of roof.

6.2 No part of roof panel may have more than 14o of angle rake. No deflector of any type on upper sides or rear of roof panel. Roof entrance hatch optional.

6.3 Maximum .75 inch ridge down sides of roof.

6.4 Maximum .75 inch rear roof stiffener (must face down).

6.5 Driver roof hatch allowed.

6.6 Maximum 1.5 inch rolled down rock guard allowed on roof front.

7 MODIFIED LITE REAR UPPER QUARTER PANEL (UMP/DIRT Figure #3) (IMCA Figure #7)

- 7.1 20" min 38" max rear upper side panel may contain plexiglass.
- 7.2 Side panels maybe no higher than 3" at the rear of the straight line of quarter panel top edge.
- 7.3 Side panel may be no higher than an imaginary line from the rear edge of roof panel to 3" above the top edge of the rear quarter panel.

8 MODIFIED LITE (IMCA/UMP/DIRT) SIDE WINDOW

- 8.1 Driver and passenger side windows must have 18" min side opening length.
- 8.2 10" minimum side opening height front and rear.

9 MODIFIED LITE INTERIOR (UMP/DIRT Figure #4) (IMCA Figure #8)

- 9.1 46" min 50" max - width of rear shelf and body panels.
- 9.2 Rear shelf may have no more than a 5° max rake and may not be concave or convex in design. No lips or spoilers of any kind.

10 MODIFIED LITE ADDITIONAL INTERIOR SPECIFICATIONS (DIRT Figure #4 - IMCA/UMP Figure #8)

- 10.1 DIRT Car only 36" min 56" max - length of right side pod area with max 30°. Left side pod may be altered for driver access. Side pods not permitted on IMCA/UMP body style.
- 10.2 Rear quarter distance from center of rear 20" min 30" max.
- 10.3 Cockpit cover with a max of ½" lip will be permitted providing the on/off cutoff switch is reachable from outside the car by safety personnel.
- 10.4 Deck lid height from ground 30" minimum, 36" maximum with NO rear lip extending upward.
- 10.5 DIRT Side pod may not extend rearward past the centerline of rear axle.
- 10.6 Body must be centered on chassis.
- 10.7 If windshield is used, it must be flat
- 10.8 All body dimensions will be tech'd + or - ½" unless otherwise specified.

11 MODIFIED LITE REAR SPOILER (DIRT Figure #4 - IMCA/UMP Figure #8)

- 11.1 A one piece, aluminum/lexan spoiler with a maximum height of 4"-inches from the rear deck will be permitted.
- 11.2 The rear spoiler must be non-adjustable from the cockpit and/or during racing conditions. Hinges, adjuster(s), slides and/or any other adjusting type device will not be permitted. May not extend the rear quarter panel.
- 11.3 Metal gurney and/or table and/or flanges and/or lips will not be permitted.

11.4 A brake and/or bend on the top of the aluminum/lexan spoiler will be permitted for reinforcement.

11.5 The maximum overall height of the spoiler when measured from the ground must not exceed 40"-inches.

11.6 No vertical supports for the purpose of fastening the spoiler to the rear deck will be permitted.

12 MODIFIED LITE REAR FRAME AREA (SEE FIGURE A ON FOLLOWING PAGE)

12.1 The rear frame area from the rear shelf downward may have an optional enclosed panel.

12.2 Louvers and holes allowed

- A. 42" min 52" max from ground -total car height without driver.
- B. 128" maximum total length, bumper to bumper.
- C. 84" max -Door and rear quarter length.
- D. 30" max height of quarter panel.
- E. Roof entrance hatch optional. With level must have no more than 1" clearance at rear roof and 10" at top front
- F. 18" min side opening length. 10" minimum side opening
- G. 73" Wheel base plus or minus 1"
- H. 128" Maximum total length to bumper
- I. No part of nose section in of spindle centerline be wider than a maximum of 24". Hood may taper out to door panels at the rear.
- J. 40" min 48" max -length of roof panel. 36" min 42" max- width of roof.
- K. 4" max
- L. The rear frame area from the rear shelf downward may have an optional enclosed panel.
- M. 30" min 38" max- side height of rear quarter panel from ground. 30" max height of quarter panel.
- N. The maximum overall height of the spoiler When measured from the ground must not exceed 40"-inches.
- O. 42" min 52" max from ground -total car height without driver.
- P. 46" min 60" max -width of rear shelf and body panels. 64" at bottom of doors and quarters.
- Q. Side panel may be no higher than an imaginary line from the rear edge of roof panel.
- R. Side panels maybe no higher than 3" at the rear of the straight line of quarter panel top edge.
- S. 20" min 38" max rear upper side panel may contain Plexiglas.
- T. No part of roof panel may have more than 14" of angle raked.
No deflector of any type on upper sides or rear of roof panel. Roof entrance hatch optional. 20" Minimum 27" Maximum bottom of frame.

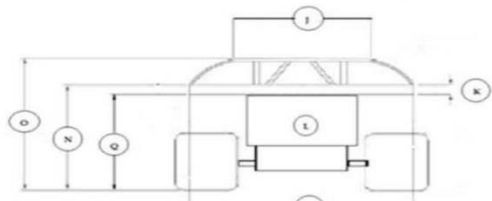
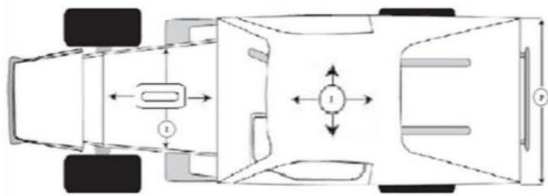
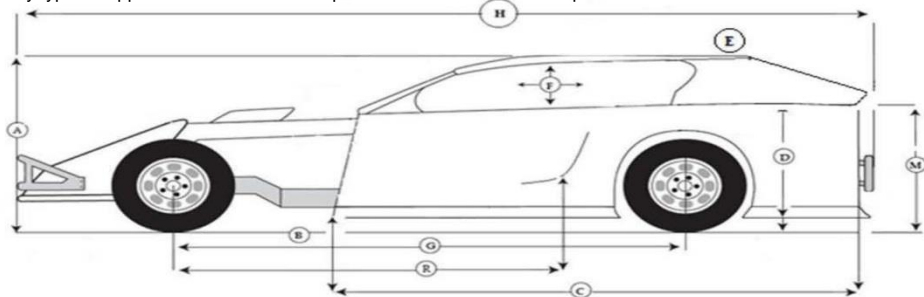


Figure 5

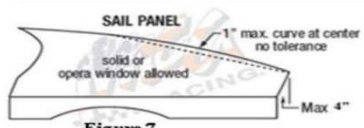


Figure 7



Figure 7

13 WEIGHT

13.1 All cars following the CMLA program must weigh a minimum of 1275 lbs, with driver after race.

13.2 No weight outside of car body, i.e. on nerf bars.

13.3 This weight program will be monitored for parity and competitiveness and may be changed at the discretion of CMLA.

14 BUMPER DIMENSIONS

14.1 Front maximum width 30", Bumper height 6" to 8" with 2-bar loop. Ground to top of bumper: 12" minimum, 16" maximum. Bumper may not extend more than 14" forward of front tires.

14.2 Rear maximum width 56" with 3 vertical bars. Bumper height 6" to 8" with 2-bar configuration. Ground to top of bumper: 12" minimum, 18" maximum. May not extend more than 10" behind rear body panels.

14.3 Bumpers must be hollow with max wall thickness of .125

14.4 Front bumper minimum width will be 20".

15 FIREWALL

Firewall must completely separate engine compartment from cockpit. Engine protruding into cockpit must be enclosed.

16 WINDSHIELD

Windshield must be Lexan, wire mesh, or protection bars. Lexan highly recommended when running asphalt. No Glass. All other cockpit areas must remain open. Deflectors may be used in front of driver. May be no wider than roll bar area.

17 NERF BARS

Full length nerf bars between front and rear tires mandatory. Maximum outside diameter of 1.25" and .125 maximum thicknesses, must be hollow and may not extend out beyond tread width more than 1" at the rear only.

18 SEAT

Seat must be high-backed aluminum, racing type. All cars will have a permanent bar located behind top of seat at shoulder level. Bottom of seat will not be more than 65" back measured from upper ball joint. Seat may be tilted back for added driver head clearance; however, no portion of the seat may be back more than 74" as measured above.

19 NUMBERS, GRAPHICS & TRANSPONDER

19.1 Car number must be minimum 12" in height and 2" stroke. Must be displayed on each door and roof or trunk. Numbers in 4" size should be on front and rear of car to help with line-ups.

19.2 See Figure #1 for decal locations.

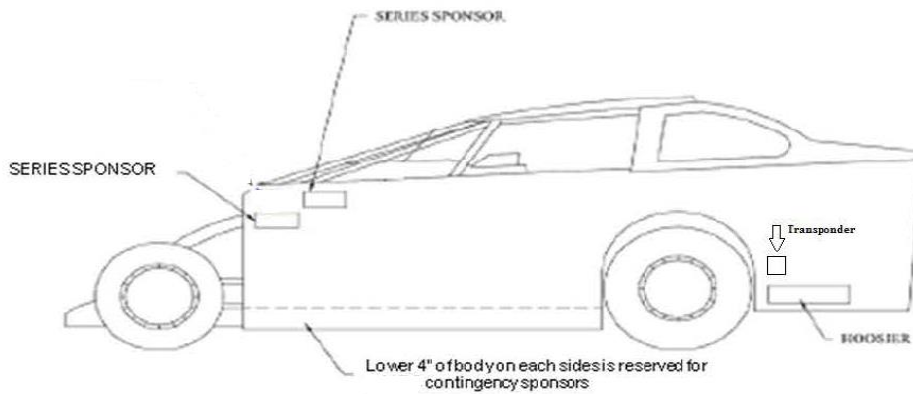


FIGURE #1

20 BATTERY, FUEL SYSTEM

20.1 Battery and fuel pump must be mounted outside driver compartment. Battery must be secure and held with non-rubber strap. Electric fuel pump allowed.

20.2 Fuel cell mandatory, maximum 5 gallons, located in trunk.

20.3 Gasoline only, no fuel additives allowed. According to local preference.

20.4 Reinforced or steel braided line must be used in all high-pressure areas.

20.5 The emergency electrical cut off switch will be clearly located and marked.

21 DRIVE TRAIN

21.1 Must use steel drive shaft from transmission to automotive rear end. Minimum 2.50" x .083" or 2" x .120".

21.2 Quick-change rear ends allowed.

21.3 Drive shaft tunnel must have minimum of 2 (3 recommended) 360-degree hoops between firewall and seat back. Must be fabricated of minimum .1875" X 1.5" steel bar. 1" X .095" steel tube highly recommended.

21.4 Rear must be locked or have a solid spool.

21.5 No titanium axles allowed.

22 FRAME AND ROLL CAGE

22.1 Mainframe members will be minimum of 1" X 2" x .120" wall rectangular steel tubing or 1 ½" DOM .095 martial non-seemed only. Mainframe rail consist of the length between the front lower a-arm cross member and the rear main roll bar upright. No aluminum allowed in cage or frame.

22.2 Mainframes will have a minimum of 3 cross members between firewall and back of seat fabricated of same material as frame rails. Alternate material may be steel angle, minimum .125" X 1.5" X 2" or 1 ½" DOM .095 martial non-seemed. Cross-bracing in floor under driver strongly recommended. Rear frame rails at rear roll bar must be 34" max and 30" minimum outside dimension. Rear roll bar must meet same dimension as the frame it is mounted to. Frame may taper forward of driver seat area. Front engine area frame width must be 24" max and 20" min outside dimension. Left and right frame rails must be the same + or - ½".

22.3 Roll cage must be DOM steel minimum 1.25" x .083" (.095 recommended) in all cars manufactured from 12/98 on. Clubs may grandfather older cars with 1" x .065" tubing. Cross-brace or diagonal strongly recommended in hoop over driver's head.

22.4 All cars must have cage bars installed across door openings on both sides. Bars should be welded in place as part of roll cage. Minimum size 1" x .065". (1.25" x .095" recommended) cars with bars mounted on doors should have full doorframe and minimum of 2 horizontal cross bars. Minimum 1" x .083". Cars with bars mounted on doors must have 1/4" x 2" steel stops welded in on each side of opening, totaling a minimum of 4" on each end of door.

22.5 It is required to have a steel bar with a minimum size of 1" X .065" from doorpost to doorpost, under the dash. Steel plate on driver's door bars recommended.

22.6 It is recommended that, if using rivets, body panels attach to tabs welded onto cage instead of drilling multiple holes into cage bars.

22.7 No down tube frame cars allowed. Any bars going from cowl area to snout must remain below the upper line of hood.

23 SUSPENSION

23.1 Front suspension must not extend rearward past the most forward panel of the firewall.

23.2 Rear trailing arm/radius rod specifications:

23.2.1 Arms may have a maximum length of 30".

23.2.2 Torque-absorbing devices are permitted.

23.2.3 Maximum 3 radius rods (trailing arms) or 3 link suspension.

23.3 Wheel offsets are allowed.

23.4 Shocks must be of all-steel construction, non-adjustable type.

23.5 No driver shall have the ability to adjust suspension from inside car.

23.6 Straight front axles are prohibited. Local clubs may grandfather existing cars for competition.

23.7 No leaf springs.

- 23.8 Fixed birdcage type rear linkage must be locked to rear end
- 23.9 Anti-roll (sway) bars permitted front or rear. Must be connected without adjustments to arm location.
- 23.10 No torsion bar suspension system of any type allowed. No lift bars.
- 23.11 No cant-a-lever or cant-a-lever type device will be permitted in or on any area of the race car.
- 23.12 Coil-over shocks are the only allowable means of supporting the chassis. One shock and one spring per wheel only. Shock and spring must remain as one unit.
- 23.13 The Bilstein SG Series 6" – 8" travel shocks are approved and recommended for use within the CMLA. Other brands are legal. There will be no adjustable shock allowed
- 23.14 NO Schrader Valves Allowed.

24 STEERING

- 24.1 All steering components must have safety fasteners such as cotter pins or self-locking nuts.
- 24.2 Manufactured quick-release steering wheel hubs mandatory.
- 24.3 Rack and pinion steering is mandatory.

25 BRAKES

- 25.1 All four wheels must have working calipers and rotors.
- 25.2 Rotors and calipers:
 - 25.2.1 Steel rotors mandatory.
 - 25.2.2 Steel OEM calipers or aluminum calipers.
 - 25.2.3 Cutting and scalloping of rotors will be allowed. No surface cracks of any type will be allowed on the face of the rotors.
 - 25.2.4 Drilling of rotors is permitted.
 - 25.2.5 Adjustable brake bias and wheel shut offs allowed.
 - 25.2.6 Competition-type master cylinders allowed.
 - 25.2.7 Shut offs allowed.

26 SAFETY EQUIPMENT

- 26.1 Fire bottle system HIGHLY recommended must be fastened down with metal spring bands and not plastic.
- 26.2 Driver uniform will consist of:
 - a) Fire suit (single layer minimum, one or two-piece).
 - b) Fireproof gloves.
 - c) Fireproof shoes.

- d) Approved helmet with SA2005 or higher.
- e) Full-face helmet.
- f) Head and neck restraint systems recommended.

26.3 Car must be equipped with 5-point safety harness, no more than 3 years old, with minimum of 3-inch belts. Tapered belts for Head/Neck restraints okay.

26.4 Aluminum, high-backed racing seat mandatory.

26.5 Window nets, arm restraints and head & neck restraints HIGHLY recommended. Window nets mandatory if arm restraints are not used.

27 WHEELS AND TIRES

27.1 Steel wheels only, minimum wall thickness .090", 13" only, Wheel width max. 8".

27.2 The official tire for the CMLA is the Hoosier plated 205/60-13, 215/60-13 and 215/60-13 medium tire, other tires to be considered by events.

205/60-13 Medium

215/60-13 Medium

225/60-13 Medium

235/60-13 Medium



27.3 Bead lock rims are optional.

27.4 Wheels Spacers Allowed

27.5 Grooving and Siping Allowed

28 1000cc MOTOR PROGRAM- MOTOR HAS TO BE STOCK

28.1 GENERAL MOTOR SPECIFICATIONS

28.1.1 The intent of the stock engine is to use engines directly from a factory supplied street legal motorcycle, without any modifications whatsoever. If there is something that has not been addressed in the above specification, do not assume modifications can be made. You must request clarification from your governing body with any item that has not been directly addressed.

28.1.2 Four cycle, four cylinders maximum only. Must have working transmission, clutch, and starter with all parts in place. Complete Clutch assembly must remain in engine. External reverse boxes allowed. No air controlled shifting devices.

28.1.3 Engines must be front mounted in engine compartment.

28.1.4 Engine setback:

28.1.4.1 The "square of the engine" (rear fins of the jugs) may not extend rearward more than 17" from the center of front spindle.

28.1.4.2 No part of the engine or transmission may extend rearward more than 21 inches from center of front spindle.

28.2 ALLOWABLE ENGINES MODEL YEARS 2013 AND OLDER OR 3 YEARS OLD

- GSXR 1000
- Honda CBR 1000 RR
- Yamaha YZFR1
- Kawasaki ZX 10

28.3 ENGINE

28.3.1 Engine parts from different year engines can be swapped between engines providing they are from the same manufacture and cc displacement.

28.3.2 No modifications to the bottom of the engine will be allowed, all bottom end components must remain in stock configuration. Charging system optional. Battery powered ignition system only. Alternator cover allowed. Factory balancer and/or balancer shaft maybe removed.

28.3.3 No auxiliary starters.

28.3.4 Engine must be cooled by original intent. May use extra fan or oil cooler.

28.3.5 Engine exhaust must include muffler and exit rearward. All muffler devices must include baffles. MUFFLER MUST MEET LOCAL TRACK REGULATIONS, (IF MANDATORY)

28.3.6 All head components must be OEM stock. After market cylinder heads and valves will not be permitted.

28.3.7 No porting, or grinding, polishing, or changing stock configuration of intake or exhaust ports allowed.

28.3.8 Engine must remain in its stock factory manufactured condition. Bore and stroke must remain unaltered from a factory manufactured specification. No increasing or decreasing of motor stroke in any way. No engine can be modified to be less than original stock OEM configuration specifications. Freshening of stock engine must only involve the replacement of any worn or faulty parts, with factory manufactured parts intended for use on that particular engine.

28.3.9 No altering of pistons, rods, head components. Crank, rods and pistons must remain OEM.

28.3.10 Header must attach directly to head in stock exhaust port location.

28.4 CAMS

28.4.1 Cams must be as per original equipment manufacturer (OEM) and meet OEM specifications.

28.4.2 Optional cam sprockets permitted.

28.4.3 Cam shafts must remain stock lift and duration and be in their original placement. (i.e. intake on intake side, and exhaust on exhaust side).

28.5 OIL SYSTEM

28.5.1 Oiling system may not include a dry sump system.

28.5.2 One piece billet, or cut down oil pans may be used.

28.5.3 High volume oil pump gear permitted.

28.5.4 The oil pan may be magnetic steel or aluminum.

28.6 FUEL SYSTEM

28.6.1 Engines must remain with the fuel injector or carburetor that was supplied with the engine from the factory. i.e. No interchanging a carburetor on an engine that came with a fuel injector. No aftermarket fuel injection and turbo chargers.

28.6.2 Carburetor/fuel injection boot must fasten directly to head in the stock intake port location.

28.6.3 After market velocity stacks permitted.

28.6.4 Factory supplied air boxes and or aftermarket air filters maybe used with OEM velocity stacks. After market air boxes, such as K & N or Kinser permitted.

28.6.5 Fuel injection casting part lines must be visible upon inspection. Intake port area may be altered to match intake boot. Alterations not to exceed 3/8" into intake port.

28.6.6 Only one fuel monitoring device can be used, but may not include any additional boxes, modules, or devices to alter motor function.

28.7 IGNITION SYSTEM

28.7.1 OEM stock ignition system only. Power Commanders up to 5 USB and or TFI may be used. Oxygen sensors or components added will not be permitted. Components added to allow traction control will not be allowed. Bazzaz fuel performance devices will not be allowed.

28.7.2 Alternator recommended but not mandatory.

28.7.3 Wiring harnesses must be left exposed for easy inspection.

28.8 FUEL

28.8.1 Any type of manufactured pumped gasoline including ethanol based fuels such as E85 will be allowed.

28.8.2 No nitro or nitrous oxide.

28.9 OTHER

28.9.1 Engine must be in production for a designated time period to determine its competitive relationship with our current motor program.

28.9.2 Any major changes to the existing engine by the manufacturer must be reviewed by the STARS Board Officials before approval.

28.9.3 All electronic and/or computerized wheel spin and/or ignition retardation and/or acceleration limiting and/or traction control devices of any type will not be permitted.

28.9.4 At NO time will there be any type of ping control devices, dial a chip controls, timing controls,

or any modifications to the ignition control boxes, distributors, or any other part of the Ignition System. This includes any add on component or components inside or outside the cockpit of any

competitors racecar. There shall be NO driver controlled wheel spin, timing or fuel delivery control devices in the cockpit area of any racecar.

29 ENGINE TECH PROCEDURES

29.2 Carburetors or Fuel Injection and all 4 spark plugs must be removed before checking compression.

29.3 After ten (10) compression revolutions of cranking:

29.4 Cylinder cranking compression- minimum 100 p.s.i.

29.5 Maximum 220 p.s.i. per cylinder average of 4 cylinders, no variance

29.6 Compression to be taken within 15-45 minutes following feature event

29.7 Measured with a Snap-On compression gauge, part number EEPV303A, with adapters EEPV304A, EEPV306A or MT26J200, EEPV305A.

30 BORO & STROKE CHART

			BORO	STROKE
GSXR 1000	988cc	2001-2002	73.0	59.0
	988 cc	2003-2004	73.0	59.0
	998.6 cc	2005-2006	73.4	59.0
	999 cc	2007-2008	73.4	59.0
	999 cc	2009-2011	74.5	57.3
	999 cc	2012-2014	74.5	57.3
	Honda CBR 1000 RR	998 cc	2004-2005	75.0
998 cc		2006-2007	75.0	56.5
999 cc		2008-2011	76.0	55.1
999 cc		2012-2014	76.0	55.1
Yamaha YZF-R1	998 cc	1998-1999	74.0	58.0
	998 cc	2000-2001	74.0	58.0
	998 cc	2002-2003	74.0	58.0
	998 cc	2004-2005	77.0	53.6
	998 cc	2006	77.0	53.6
	998 cc	2007-2008	77.0	53.6
	998 cc	2009-2010	78.0	52.2
	998 cc	2011-2012	78.0	52.2
Kawasaki ZX 10	998 cc	2004-2009	76.0	55.0
	998 cc	2010-2014	76.0	55.0

31 MOTORCYCLE ENGINE SERIAL NUMBERS

VIN codes reveal who manufactured the motorcycle, its model, engine size and other characteristics, where the motorcycle was manufactured (both country and plant), as well as the year it rolled off the assembly line.

Step 1 – The First 3 Characters

This first section renders information in this order:

- 1st, the country of origin
- 2nd, the manufacturer
- The third character will be either a “1” or an “A” to indicate “motorcycle”

Step 2 – The Next 5 Characters

The second section is called the vehicle descriptor section. The 4th place in the VIN code indicates the vehicle category:

- C = scooter
- B = business model or commuter
- N = single cylinder sport / street
- G = multiple cylinder sport / street
- F = Family
- S = off road
- V = v-type engine, street, V2/V4/V6/V8, etc

5th place is for engine displacement:

- P=600-699cc
- R=700-749cc
- S=750-849cc
- T=850-999cc
- U=1000-1099cc
- V=1100-1199cc
- W=1200-1299cc
- Y=1400-1499cc
- Z=1500cc and up

The next digit (6th place) is for engine type:

- 1=2 stroke single
- 2=2 stroke twin
- 3=2 stroke triple or four
- 4=4 stroke single
- 5=4 stroke twin

7th place goes to design sequence and 8th is for model version. The 9th space is a check digit mandated to verify the accuracy of the VIN code. This place does not render information about the motorcycle.

Step 3 – Year code and Manufacturing Plant

10th and 11th spaces go to the year code (10th) and to which plant the motorcycle was manufactured (11th). The

year codes are as follows:

- 1999.....X
- 2000.....Y
- 2001.....1
- 2002.....2
- 2003.....3
- 2004.....4
- 2005.....5
- 2006.....6
- 2007.....7
- 2008.....8
- 2009.....9
- 2010.....A
- 2011.....B
- 2012.....C
- 2013.....D
- 2014.....E

Step 4 – The Last 8 Digits

The last 8 digits are the motorcycle's serial number. These are assigned sequentially as the motorcycles roll from the assembly line.

32 WEIGHT

32.1 All cars following "1000cc Motor Program" must weigh a minimum of 1300 lbs, with driver after race.

32.2 No weight outside of car body, i.e. on nerf bars.

33 MISCELLANEOUS RULES

33.1 NO two-way radios or communication devices.

33.2 One-way raceceivers from officials only.

33.3 No mirrors allowed.

34 DRIVER QUALIFICATIONS

All drivers must be minimum of fourteen (14) years of age or track policy. All drivers under 18 years of age MUST have a signed waiver by parent or guardian. All rookie drivers under the age of 14 will have tag only status for 8 main events minimum. All drivers under 14 years old must audition for the class and final decision is up to CMLA and drivers. All Rookies must identify their cars with yellow tape.

35 GENERAL REGULATIONS

35.1 The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish the minimum acceptable requirements for such events. These rules shall govern the condition of all events, and, by participating in these events, all participants are deemed to have accepted and compiled with these rules. NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM PUBLICATION OF OR COMPLIANCE WITH THESE RULES AND REGULATIONS. They are intended solely as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator, or official. Any interpretation or deviation of these rules is left to the discretion of CMLA Officials and the decision is final. NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL

RESULT FROM SUCH DEVIATION OF SPECIFICATIONS.

35.2 No intoxicating spirits or controlled substances are to be consumed before or during race events by anyone entering the pit area of tracks.

35.3 CMLA reserves the right to reject or allow entry of any driver or car.

35.4 Abuse or confrontation of any racetrack or CMLA official and/or use of abusive language is strictly prohibited. FAILURE TO ABIDE BY THIS RULE MAY RESULT IN SUSPENSION, LOSS OF POINTS, WINNINGS, RECORDS, AND/OR CASH FINE.

35.5 Any disputes involving qualifying times, lap scoring, finishing position is under the jurisdiction of the hosting track officials. CMLA officials will address winnings, points, or rules interpretations. Any disputes or discussions of this nature are to be held with CMLA officials only.

35.6 Drivers, owners, and/or crew shall have no claims or actions of any kind against or cause of action for damages of any kind, expenses, or otherwise against CMLA, any track owner, promoter, or official by reason of disqualification or damage to either car, driver, or both. This regulation shall be deemed a covenant not to sue

made by each driver, owner, and/or crewmember to CMLA, any track owner, promoter, or official with respect to any alleged act or omission or agreement by any of him or her.

39.7 CMLA or track officials may inspect any car any time for safety, mechanical, or rules compliance may inspect any car.

39.8 CMLA HAS THE RIGHT TO CHANGE THE RULES AT ANY TIME PROVIDING THE CMLA BOARD AGREES TO THE CHANGES